

	<h1>Information bulletin</h1>	Doc.-No. 019.17.001
		Revision 0.0
		Date 2017-06-01
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Record of Revision		
Rev. #	Issue date	Change description
0.0	01/06/2017	Initial issue

	Name / Function	Date	Signature
Prepared	A. Abromavius Office of Airworthiness	01/06/2017	
Approved	K. Juo as Chief designer	01/06/2017	

1. Planning Information

1.1 Effectivity

LAK-19, LAK-19T sailplanes.

Serial numbers: 001, 004, 005, 011, 015, 019, 021, 022, 029, 032.

EASA-Type certificate:


EASA TCDS No. A.012, Issue 03, 16 October 2006

1.2 Reason

The wheel brake control lever (on the control stick) may touch the ventilation handle or other switches on different instrument panel layouts. More attention must be taken for the sailplanes, with instrument panels which varies from default LAK-19, LAK-19T panels and are manufactured or installed not by the JSC "Sportin aviacija ir Ko".

1.3 Compliance

Clearance between the wheel brake lever's and the instrument panel's components must be checked after receiving the information bulletin.

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1.4 Approval

The technical information contained in this Service Bulletin has been approved under the authority of the design organisation EASA.AP160.

1.5 Special Tools and Materials

There is no need for any special tools or materials.

1.6 Reporting

Compliance of this Bulletin must be reported to JSC "Sportin aviacija ir Ko" customer support department.

2. Accomplishment Instructions

CAUTION: Obey the safety Precaution and the General Maintenance Practices.


2.1 Preparation

To check for the control stick movement, make sure that there are no any foreign objects in the cockpit that can limit the control stick motion.

There is no need to assemble the sailplane, because inspection must be done in the cockpit. But if you are checking the assembled sailplane, make sure that the ailerons are free for moving upward and downward (doesn't touch the ground or other objects).

2.2 Inspection

- 1) CLOSE the ventilation by pushing the handle and turn OFF all switches on the instrument panel.
- 2) Move the control stick to the most forward position and turn it from the left to the right in the full range of motion and opposite - from the right to the left. Make sure the wheel brake is released, the control lever is in its forward position. Check if the wheel brake control lever doesn't hit or touch ventilation handle or any other switch on the instrument panel. See the Figure 1. and Figure 2. for the detailed explanation.
- 3) OPEN the ventilation by pulling the handle and turn ON all switches on the instrument panel.
- 4) Repeat the inspection by moving the control stick as described in paragraph 2.

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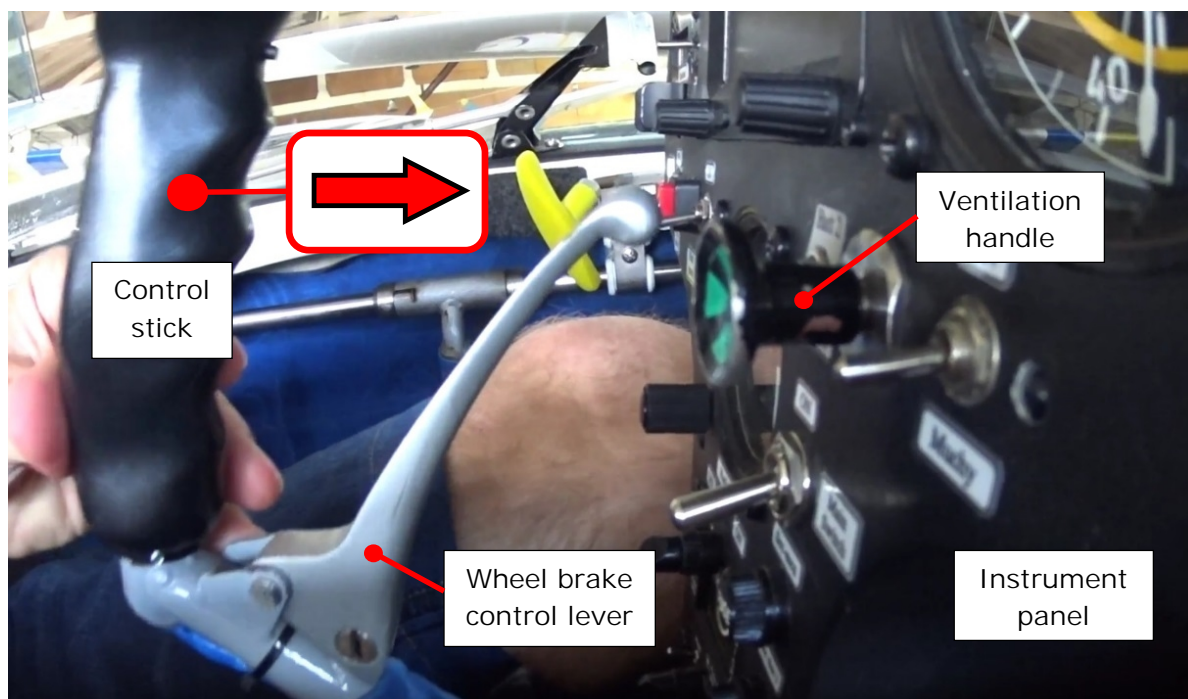


Figure 1. Move the control stick to the most forward position (direction of the arrow)

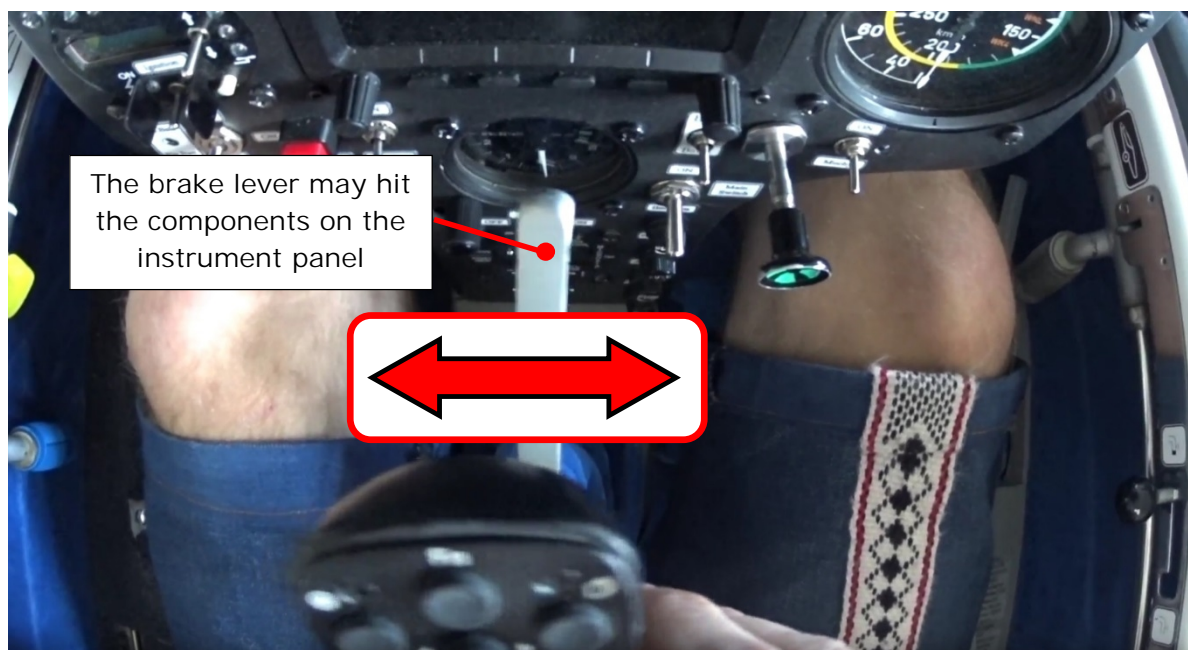



Figure 2. Turn the control stick in both sides in the full range of motion

It is the operators responsibility to comply with the relevant aviation regulations of the country in which the product is registered and operated.

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2.3 Close-up

- 1) You must inform manufacturer with the results of inspection.
- 2) If you find that wheel brake control lever is hitting some components on the instrument panel, there is important to make a modification approved by the manufacturer.